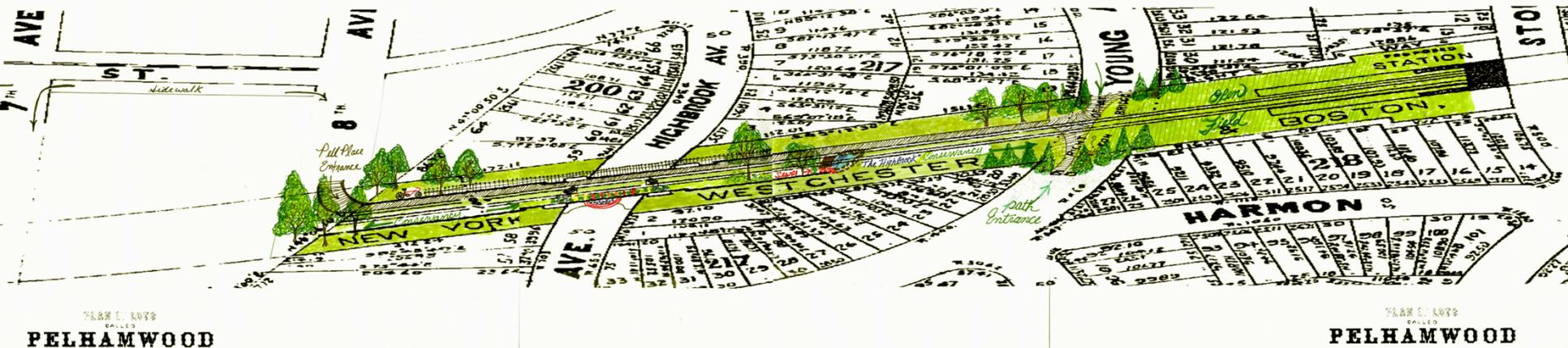
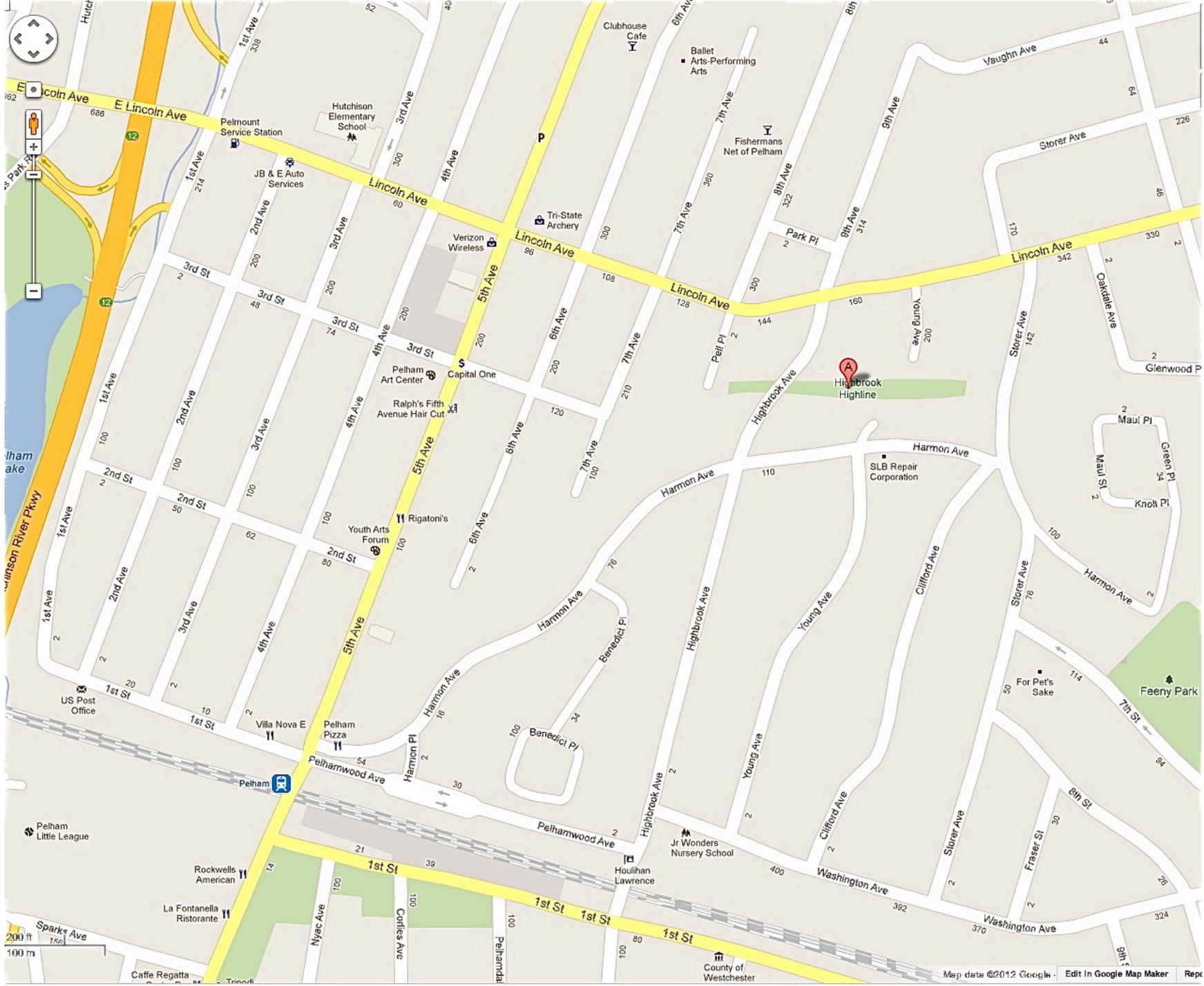


NYW&B Parkland Concept

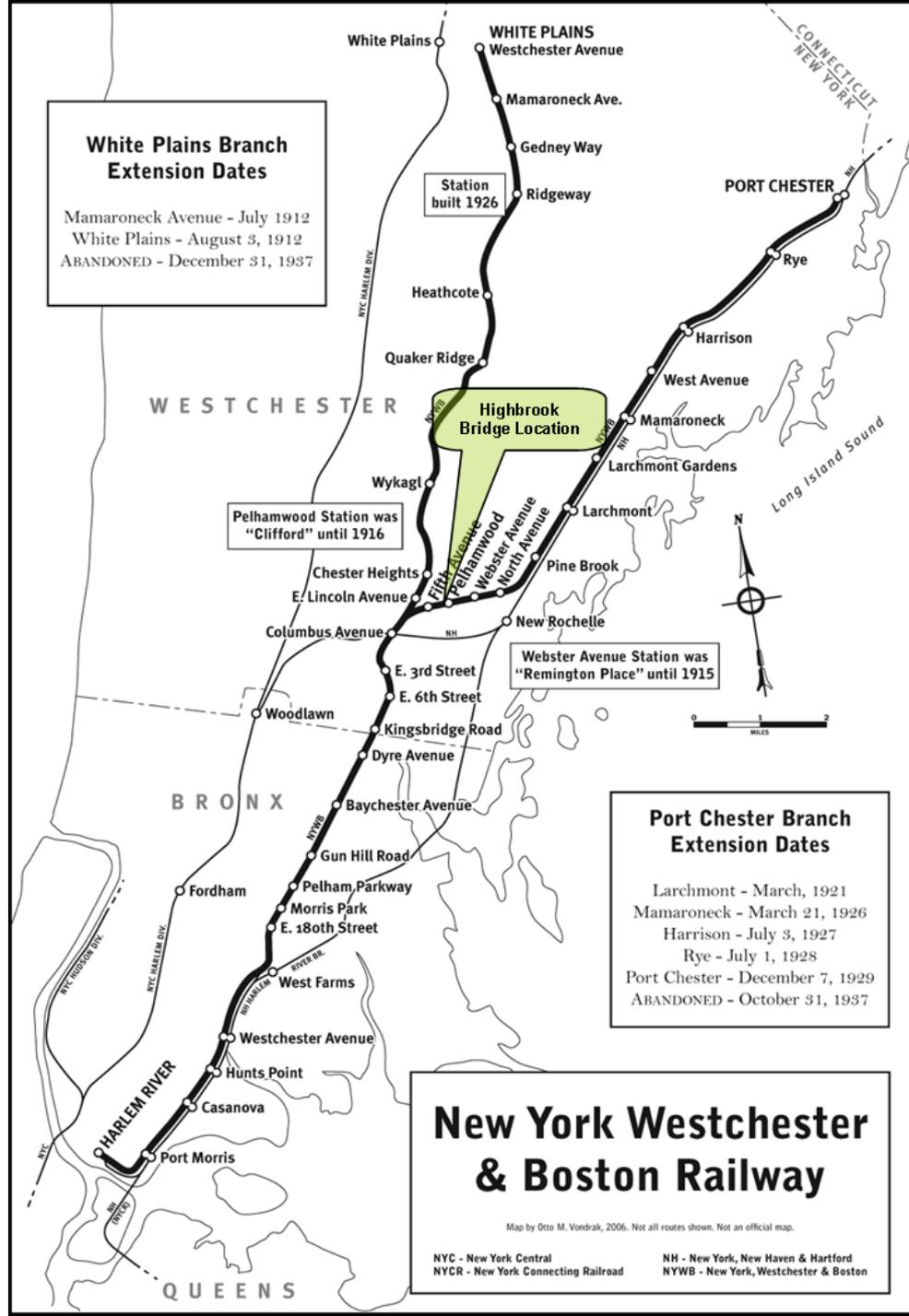


Village Of Pelham Board Meeting
June 16, 2015



White Plains Branch Extension Dates

Mamaroneck Avenue - July 1912
 White Plains - August 3, 1912
 ABANDONED - December 31, 1937



Pelhamwood Station was
 "Clifford" until 1916

Webster Avenue Station was
 "Remington Place" until 1915

Port Chester Branch Extension Dates

Larchmont - March, 1921
 Mamaroneck - March 21, 1926
 Harrison - July 3, 1927
 Rye - July 1, 1928
 Port Chester - December 7, 1929
 ABANDONED - October 31, 1937

New York Westchester & Boston Railway

Map by Otto M. Vondrak, 2006. Not all routes shown. Not an official map.

NYC - New York Central	NH - New York, New Haven & Hartford
NYCR - New York Connecting Railroad	NYWB - New York, Westchester & Boston

Other NYW&B Railway Projects (New York Westchester and Boston)

Heathcote Station in Scarsdale



completed

Quaker Ridge Station in New Rochelle



completed, now a private home

White Plains Greenway

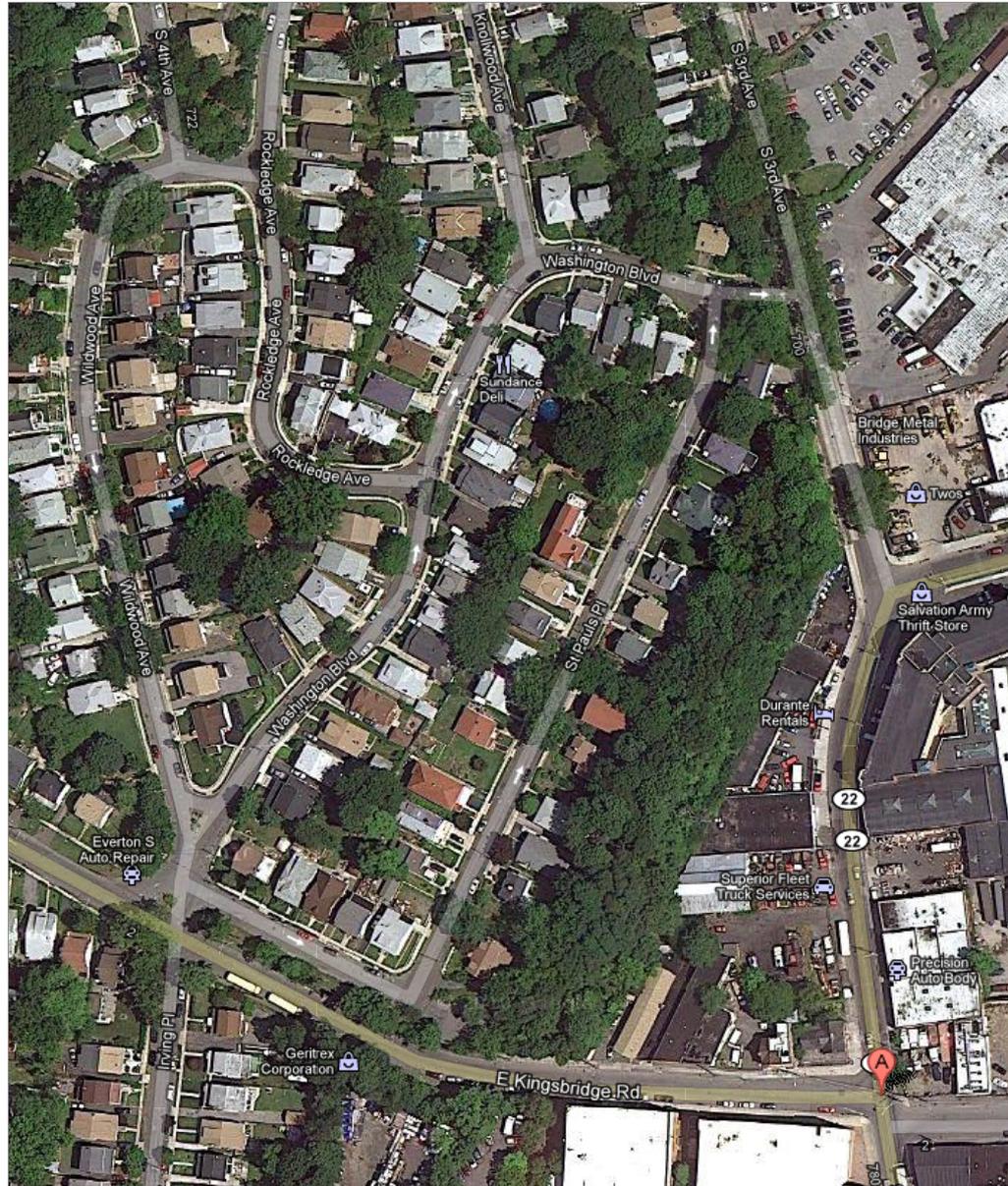


Morris Park/180th Street



a New York City Landmark

Kingsbridge, Mount Vernon, NY





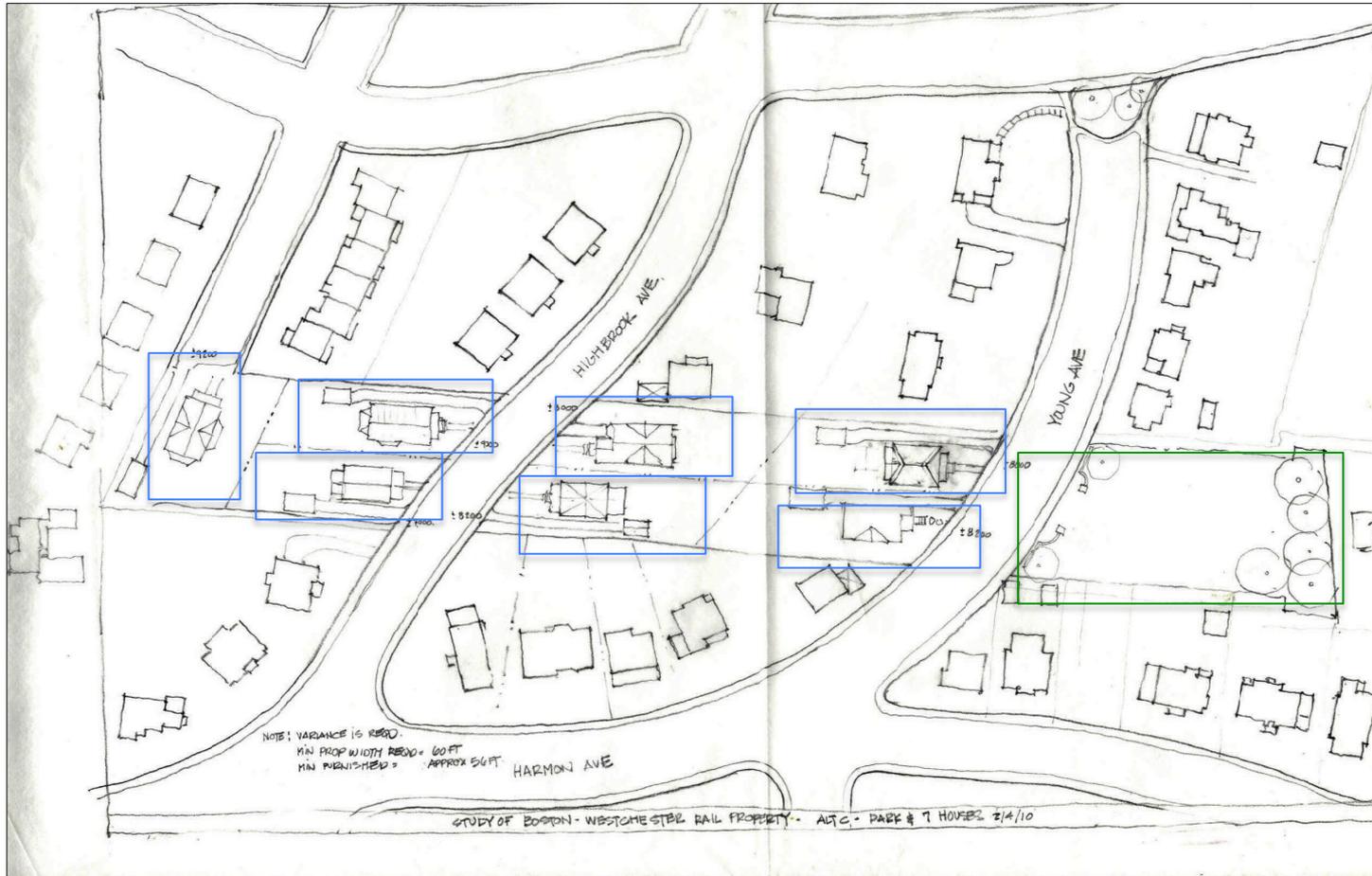
Why Parkland

- Increase green space in Pelham
 - 22% of open space in the village
 - Real estate analysis - Pelham lacking in green space/parks
- Fund Maintenance of property including bridge
 - Modjeski and Masters bridge study (2012)
 - Westchester Land Trust
 - Elected officials Senator Klein, Congressman Engel
- Historic Eligibility
 - Significant history
 - Further leverage funding

Considerations

- Create a neighborhood amenity
 - Passive park, no permanent equipment
 - Wooded path, gardens, make brook area safe
 - Increase the value of adjoining homes
- Preserve green space
 - Prevent overdevelopment of Pelham
 - Mitigate flood zone on Highbrook Avenue
- Keep road option open for future consideration
 - Connection of Young Avenue

Housing Plan from 2010/Young Ave Continuation



Committee's Conclusion

Best option is to refine the Greenway concept

Supported by the Pelham Greenspace Committee's Jan 2010 report

- mandate was to explore sale of the property or dedication of the land as permanent green space
- advantages outweighed the disadvantages when the sale and the use of the green space were evaluated
- consistent with 2008 Master Plan

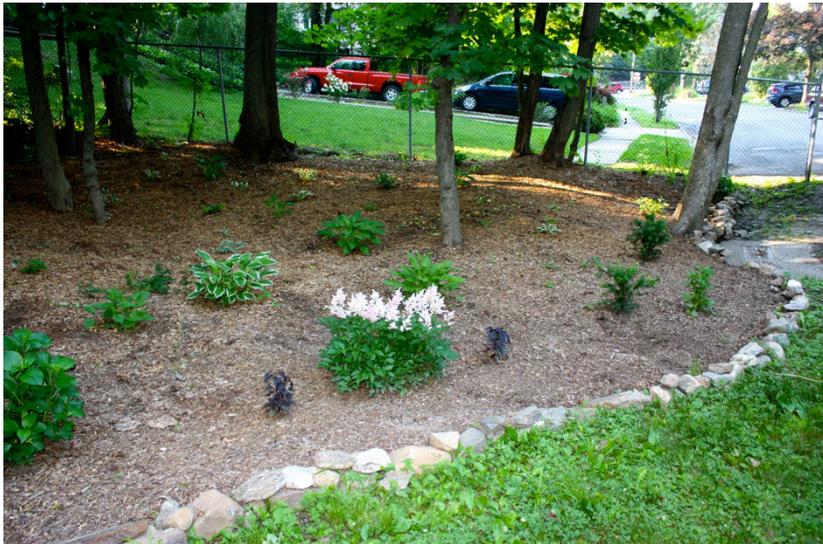
“Village should seek to provide a specialized and diversified shopping experience in a vibrant, relatively dense downtown, while maintaining the lower density of its residential districts,” pg 7.

“The Village's neighborhoods – Chester Park , Pelville, Pelhamwood and Pelham Heights - all have special architectural and topographical features and should be preserved,” pg 19.

NYW&B Milestones

Jun 2011	JN article, radio and TV interviews
Dec 2011	Sierra Club endorsement
Mar 2012	Determination of eligibility letter JLP Community Grant recipient
Apr 2012	Planet Day of Service
May 2012	Field opens (Phase 1) 100 th Anniversary celebration NYW&B
S/S 2013	Made in the shade planting party, arborvitae, bulbs
S/S 2014	Mulch, fence removal, trees planted, bulbs, rock lined gardens, benches, Dorothy Perkins roses

Young Avenue Field





**New York State Office of Parks,
Recreation and Historic Preservation**

Historic Preservation Field Services Bureau • Peebles Island, PO Box 189, Waterford, New York 12188-0189

518-237-8643

www.nysparks.com

Andrew M. Cuomo
Governor

Rose Harvey
Commissioner

March 27, 2012

Susan P Muttti
Friends of the Highbrook Highline
62 Highbrook Avenue
Pelham, NY 10803

Dear Ms. Muttti:

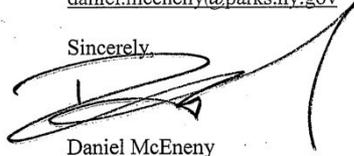
Thank you for contacting the New York State Office of Park Recreation and Historic Preservation and providing us with the finely prepared request for National Register eligibility. Based on the information submitted, it is the opinion of the Division for Historic Preservation that the Highbrook Avenue Bridge in the Village of Pelham, New York is eligible for listing on the National Register of Historic Places. The bridge was constructed from 1910 to 1911 for the New York, Westchester & Boston (NYW&B) Railway, an electric commuter line operating from the Harlem River in the Bronx to White Plains and Port Chester. The interurban line played a substantial role in the suburban development of Westchester County from a period of 1912, when the line opened, until 1937 when it closed during the Great Depression. The Highbrook Avenue Bridge stands as a significant resource articulating how Pelham's physical environment was directly shaped as a result of railways and specifically the NYW&B. During the line's peak years of operation, the largely commuter population of Pelham nearly quadrupled, creating what many have described as New York City's first true bedroom community.

The bridge is locally significant from a period of 1910, the date of its initial construction, to 1937 when the railroad shut down service. It meets the criteria for National Register evaluation under Criteria A in the area of Social History for its previously noted association with the development of Pelham during the period and under Criteria C in the area of Design as an example of reinforced concrete construction designed by Alfred Fellheimer. A noted architect whose work is represented in structural work found in the design of New York City's Grand Central Station, two previously National Register listed rail buildings from the NYW&B (of which he served as the principal architect for the line) and countless other example throughout the United States, his contribution to the

field of architecture is best articulated in the numerous railroad structures he designed for several lines during the early-Twentieth Century.

At this time the eligibility for the National Register covers the 1.93 acre site of village owned property identified in the applicant's request. Also noted in the request is the presence of a buried platform once associated with the bridge and rail line. It should be noted that at this time, the eligibility does not cover the now filled in platform and that the boundaries identified are preliminary until a proper site visit can be coordinated at the Village's request. We also strongly encourage you to continue to work with the Village to facilitate the listing of this wonderful resource, which requires the support of the owner. It should be noted that listing on the Register may open the door for potential funding sources for restoration. If you or the Village has any questions, please do not hesitate to contact the office at 518.237.8643 x3257 or by email at daniel.mceneny@parks.ny.gov

Sincerely,

A handwritten signature in black ink, appearing to read 'Daniel McEneny', written over a horizontal line.

Daniel McEneny
Historic Preservation Program Specialist
New York City, Long Island, & Westchester County

Cc: Robert Yamuder, Village of Pelham (via email)
Carol Desmond, President, Pelham Preservation & Garden Society (via email)



LOWER HUDSON GROUP

c/o George Klein
74 Croton Dam Road
Ossining, NY 10562
(914) 941-2505

December 19, 2011

Village of Pelham Board of Trustees
195 Sparks Avenue
Pelham, NY 10803

Re: Highbrook Highline

Dear Mayor Hotchkiss and Board of Trustees:

We are writing to express our support of a greenway in Pelham on the site of the NYW&B railway. The Sierra Club became aware of this project through a presentation by the Friends of the Highbrook Highline. Converting this abandoned 2 acre parcel into a usable place for the community is visionary, a proven "highest and best use" option in similar communities, and is consistent with our mission at the Sierra Club to "Explore, Enjoy and Protect the Planet."

The combination of a field for unstructured play, a wooded path and an elevated arch offer diverse opportunities for people to interact with the outdoors. There is ample undisturbed ecosystem already in place to design and reuse as an educational resource. The land has the potential for use by people of all age groups. The historical aspect of the arch affords an additional connection to a place in time.

The Sierra Club has also endorsed a similar project in Mount Vernon, Kingsbridge Mound, on the same railway. Though the properties appear small in scale, they are large compared with the relative size of the municipalities. For Mount Vernon, 1.5 acres in a city of 4 square miles, the preservation of the last parcel of forest is significant. As is the case in Pelham, it is supported by neighbors as well.

We believe that partnering with other civic and volunteer groups in Pelham to effect this new use will result in a space that is well planned, valued, cared for and celebrated by the community.

We look forward to hearing about your land use decision in the future.

Sincerely,


George Klein
Chairman

PETER PENNOYER AND ANNE WALKER



THE ARCHITECTURE OF
Warren & Wetmore

FOREWORD BY ROBERT A. M. STERN

Next Steps

- Continue discussion
- Research legal aspects/implications
 - Parkland
 - Historic eligibility

Committee Chairs/Work
(2011 – present)

Dr. Roger Wines, Professor Emeritus, Fordham University

- Prepared the historical eligibility application

Ann Dumas-Swanson

- Land Use research and liaison to Westchester Land Trust

Sue Seither

- Engineering, liaison to Modjeski and Masters
- Archives and research, historical photos and other materials





HIGH LINE

The Inside Story of New York City's
Park in the Sky

Joshua David and Robert Hammond

 Farrar, Straus and Giroux
New York

NOVEMBER 30, 2011

To Susan,

WISHING YOU BEST OF
LUCK WITH THE HIGHBROOK
HIGHLINE!

